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Introduction

As stated in the previous chapters, quality of life and economic development are goals of this Comprehensive Land Use Plan. Maintaining quality of life for residents and property and business owners while fostering employment is paramount in order to slow or reverse the loss of talented youth taught by local schools, another Town goal.

This ability to maintain the quality of life and the qualities that the Town and its communities enjoy holds the key to open many development opportunities. Orleans's character, quality of life and outstanding beauty continues to make it a desirable place to live, work, and visit.

This Comprehensive Land Use Plan provides a general understanding of the factors that

will shape the development of the Town: the Route 12 and 180 corridors, LaFargeville, Fishers Landing, Stone Mills, Wellesley Island, Interstate 81/NYS 411 corridor, Perch River and areas in-between.

Town Land Use

This section of the Comprehensive Land Use Plan is the Town's opportunity to make recommendations concerning the quality, character, and density of new development that may occur as a result of the new and anticipated infrastructure.

Under the "home rule" provisions of the New York State Constitution, local municipalities, based on local Comprehensive Plans, have the authority to regulate land use through zoning and related regulations. This Comprehensive Land Use Plan should be considered the basis for local land use considerations as well as capital improvements, infrastructure and zoning law amendments.

Orleans Vision and Goals

The following Town vision and goals serve as broad mission statements and directions that Orleans residents feel are important to aim for and attain. The vision and goals are primarily related to the quality of life, new development and land use planning. The subsequent objectives are more specific and address various components of each goal. The following vision, goals, and their associated objectives were developed by the Planning Committee. They are based upon the plan inventory information and guidance

provided by the community, which was generated through the entire planning process. This public input process consisted of a public input survey, and several public meetings. The goals and strategies were developed to be included in the Town's Comprehensive Land Use Plan in order to provide the Town with direction and guide community enhancement into the future.

In conclusion, the vision and goals served as the foundation from which the subsequent objectives of the Plan were devised. Elements of the objectives in the Plan should be developed, worked upon and implemented in order to achieve elements of the vision and the associated goals.

The Town hopes that the following vision, goals, and objectives provide local leaders and developers with guidance to improve the Town's economy, character, transportation systems, quality of life and niche in the regions overall economy. Orleans ambition is that future growth

contributes toward the goals and objectives in an appreciable way to advance the Town and its hamlets and neighborhoods to realize its vision.

The following pages outline the Town Goals and Objectives meant to take steps toward the Town's vision. Over time, implementing the goals and objectives will help the Town fulfill and realize its vision. Doing so involves a number of actions in order to keep the Town moving toward its vision.

In some cases, more cooperation will be needed, or additional work by a potential committee or existing board might be called for. Still further, additional tool or tools may need to be developed which could then be used by the Town in its capacity to affect the desired change over time. Lastly, some private investment may be required to affect change where growth is desired to allow the Town Vision to be realized.

Orleans Town Vision

"Town residents enjoy Orleans spectacular scenery, outdoor recreational opportunities, abundant rural character, openness and waterfront lifestyles that attract seasonal and year-round residents year after year. Its hamlets serve as home, job centers and gathering places. Its seasonal homes provide an influx of summer residents and source of seasonal opportunity. Orleans treasured rivers, farms and open spaces have served as its lifeblood for generations. The Town would like to maintain its unique environment/quality of life for current and future residents, property owners, retirees and visitors while fostering investment to allow businesses and farms to flourish while enticing employment opportunities."

Land Use/Character Goals:

1. Foster future land uses in a manner and layout that will preserve rural character, agricultural and/or natural features.
2. Encourage commercial and related development that is compatible with current scale of development and town character.
3. Provide for residential development in a manner that results in both housing and neighborhoods that are safe, healthy, and attractive, while providing choices and supply of a variety of dwelling units.
4. Consider compatible renewable energy systems in appropriate locations identified by the community.
5. Foster walkable pathways to expand physical or visual access to the waterfront and/or recreational areas.
6. Preserve the scenic character of community gateways and corridors along NYS Routes 12, 180, and I-81 within the Town.
7. Pursue property maintenance and zombie property initiatives (with partner agencies) to maintain or enhance community character.

Transportation Goals:

1. Provide pedestrian connectivity within parking areas, between project parking lots, and along corridors.
2. Enhance pedestrian options along priority roads and highways to foster safety and limit vehicle/pedestrian conflicts.
3. Fill gaps in sidewalk systems and extend such pathways to connect to pedestrian destinations such as schools and parks.
4. Consider arterial highway and collector road standards to maintain their function while fostering economic growth.

Community Facility Goals:

1. Incorporate project components to make it safer and easier to walk to, from and within projects.
2. Identify sidewalk gaps, crosswalk needs, and connections that should be pursued to enhance walkability & safety within hamlet and community areas.
3. Identify municipal water and sewer needs so funding sources can be identified and pursued.
4. Maintain the outstanding educational and recreational facilities for Town/District residents.

Natural Resources Goals

1. Preserve and enhance open space, farmland, natural beauty, and critical environmental areas that provide scenic quality, help define rural character, waterfront community character, and recreational opportunities.
2. Encourage green infrastructure to preserve water quality, air quality, and increase community resiliency to storm events.

Economic Development Goals

1. Foster economic development that capitalizes on rural character, small town character and riverfront character reflected along the St. Lawrence River shorelines and unique communities.
2. Use a balanced approach to increase job opportunities and expand the local economy while preserving quality of life.
3. Foster local employment opportunities to help retain the area's young adults.

LAND USE CHARACTER OBJECTIVES:

1. Capitalize on the NYS Rte 12 Corridor economic development opportunities while preserving/enhancing its rural and scenic character.
2. Initiate zoning provisions to foster future residential, commercial, and other types of development in appropriate locations and compatible scale (in terms of its size, height, bulk, intensity and aesthetics to its neighborhood).
3. Encourage the preservation of productive agricultural lands, farm operations, and farmland protection in priority areas.
4. Utilize character areas to enable Town priorities to be incorporated into the zoning law.
5. Encourage the placement of land uses that are compatible with the surrounding development, natural features, historic and/or archeological features.
6. Develop appropriate standards to ensure future commercial and non-residential development is in keeping with current scale of development and applicable rural or urban town character.
7. Direct more intensive types of future development into areas where public facilities and services can be economically provided in the future, as needed.

TRANSPORTATION OBJECTIVES:

1. Seek funding sources to fill sidewalk gaps, create crosswalks, and provide walkable pathways where needed (especially to connect housing areas to school, parks, and other key areas to enhance safety and community walkability).
2. Consider an open space or coverage requirement along arterial highways to limit access drive frequency, maintain the areas unique rural character and views of the landscape and promote interconnected sites.
3. Protect arterial highway and collector road function by fostering shared access drives and limiting curb cut abundance.

COMMUNITY FACILITY OBJECTIVES:

1. Promote infill development by preserving, upgrading and reusing existing properties.
2. Consider requiring green infrastructure design for stormwater facilities within flood prone areas.
3. Encourage green infrastructure design at the site and neighborhood level to mitigate susceptibility to localized flooding.
4. Analyze walkability within LaFargeville and other community areas to identify pedestrian needs to enhance safety.

NATURAL RESOURCE OBJECTIVES:

1. Encourage the preservation of natural features, open space and water quality within area rivers, creeks, and wetlands.
2. Protect important wildlife habitat, as well as threatened, endangered, and species of concern.
3. Preserve the quality and natural state of environmentally sensitive or critical areas, including surface waters, groundwater, wetlands, forested land, and wildlife habitat and migration corridor and stopover sites.

ECONOMIC DEVELOPMENT OBJECTIVES:

1. Capitalize on community character to expand economic development opportunities.
2. Expand the local economy in a sustainable way that balances growth and preserves the natural environment and quality of life for residents and visitors.
3. Support economic development that will expand summer tourism to the shoulder seasons as well as year round opportunities for residents and visitors.
4. Consider marketing the Hamlets and Town as a residential destination to expand development opportunities.
5. Consider marketing Orleans diverse communities for their unique quality of life, scenic character and specialness.
6. Develop local labor market to attract employers

Introduction-Planning Project Considerations

The following Planning Considerations should be considered for use when reviewing development and redevelopment projects, updating zoning standards used to review projects, and establishing subdivision requirements which establish minimum standards for lot creation, road design, and ultimately, the pattern of development for generations. They are the product of discussion and thought about how development can respect the area and be shaped to complement the character of the Town and its hamlets and ultimately improve the quality of life for current and future residents.

Overall Town Planning Project Considerations

- ✓ **Future growth potential** - explore funding opportunities to expand sewer and water district areas and local capacity. Identify appropriate areas within the Town and Hamlets for suitable population and employment growth. Increasing capacity could make infill projects more feasible and limit excessive cost of expansion of lines and districts.

For example, appropriate areas for residential, commercial, or mixed use zoning districts should be identified and established in order to enhance development opportunities, coordination and allow services to be provided to reinforce such areas to locate additional growth. The area along the Route 12 corridor, where several businesses are concentrated could be

considered a business district. Similarly, certain areas where housing is concentrated should be considered for establishment of residential zones, thus reinforcing and protecting such uses. This would also allow needed services to be examined for feasibility. Also, areas with services could be targeted for residential growth in suitable densities.

- ✓ **Attracting growth** - foster appropriate development and infill opportunities that interconnect with existing neighborhoods and business areas that improve the overall desirability and destination quality of the Town.
- ✓ **Curb cut/access management** - shared driveway accesses and internal access connections among adjacent businesses are favored over excessive numbers of individual curb cuts with no connections.

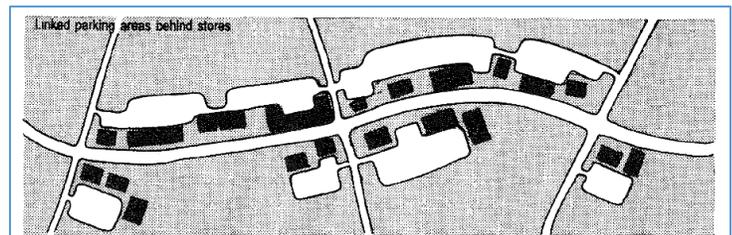


Figure 9-22. Connecting rear parking lots allows customers to drive to many other shops in the corridor without re-entering the highway and interrupting traffic flow. Such arrangements can be required for new development, expansion of existing buildings, and redevelopment.

- ✓ **Connecting current businesses** - as new projects occur, foster traffic and pedestrian connections to existing business and residential areas and allow improvements that will improve existing business competitiveness, alleviate congestion, and enhance aesthetics.
- ✓ **Infill** refers to the development of vacant parcels within previously built areas. These areas are typically served by public infrastructure, such as transportation, water, wastewater, and other utilities.

✓ **Drainage affects water quality** - drainage facilities should be incorporated onsite and existing drainage systems should be improved/upgraded or maintained to limit storm water impacts downstream or on neighboring properties. Such drainage facilities should include detention and retention, bank stabilization, and safe practices for snow removal and lawn care to keep particulates and contaminants from draining into local water bodies.



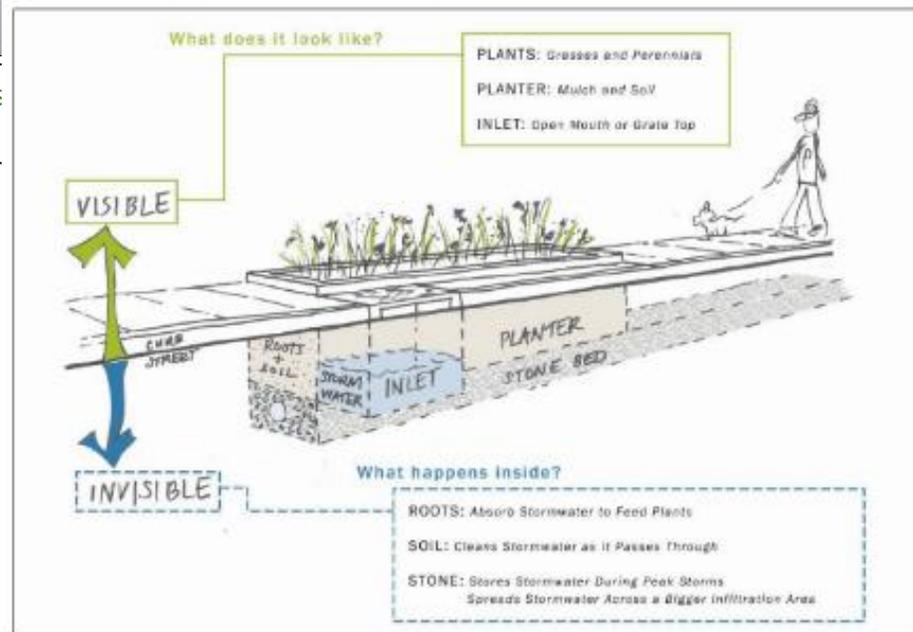
Surface water flow example: illustrates common pathways that contaminants can be transported by surface water flow

For example, any substance within the watershed which can be transported by water (e.g. detergents, eroded soil, septic effluent, pesticides, & oil/road dust) can eventually reach the river, stream or lake and affect water quality. It is not only shorelines uses, but activities anywhere within a lake or stream’s watershed which affect water quality.



Rain Garden (also known as a bioswale) captures stormwater as it flows down the street.

✓ **Green Infrastructure** – Rain gardens or bio-swales can be incorporated in medians and along parking lot perimeters. Benefits of which include less water runoff, heat island mitigation, and a more walkable and pedestrian friendly environment. Permeable or porous pavements also reduce runoff most of the year and can be used to limit the size of retention areas and drainage basins.



What a bioswale looks like underground. (Source: NYC Department of Environmental Protection.)

- ✓ **Historic character street layout** - consider requiring new development areas to extend the grid pattern with blocks and multiple connections to maintain traffic flow and access.

For example, a lack of a grid pattern can funnel too much traffic from a side road onto the main traffic artery which often creates a congestion point. However, extending the grid pattern allows flow from several side roads at once, which typically softens the overall traffic impact.

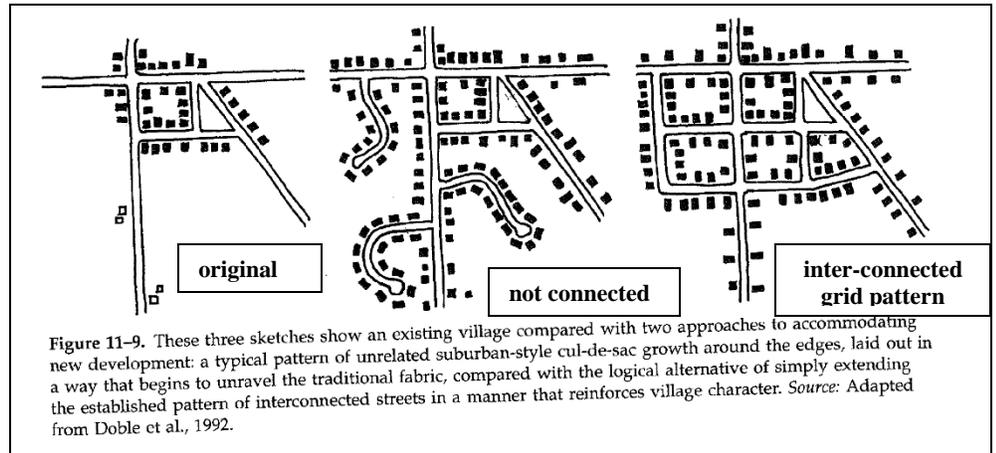


Figure 11-9. These three sketches show an existing village compared with two approaches to accommodating new development: a typical pattern of unrelated suburban-style cul-de-sac growth around the edges, laid out in a way that begins to unravel the traditional fabric, compared with the logical alternative of simply extending the established pattern of interconnected streets in a manner that reinforces village character. Source: Adapted from Doble et al., 1992.

- ✓ **Historic building form & styles** - Where appropriate, consider guidelines for historic compatibility when new developments are proposed and when reuse of existing buildings and homes occurs.



For example, within historic districts or areas with a predominant style, form, scale, and character, new structures should be required that echo the scale, style, form, rhythm and character of the neighborhood. Don't put a one story building in a three story block or a concrete-sided building on a street of wooden

sided buildings. Consider consistency with size and materials whenever a new building is



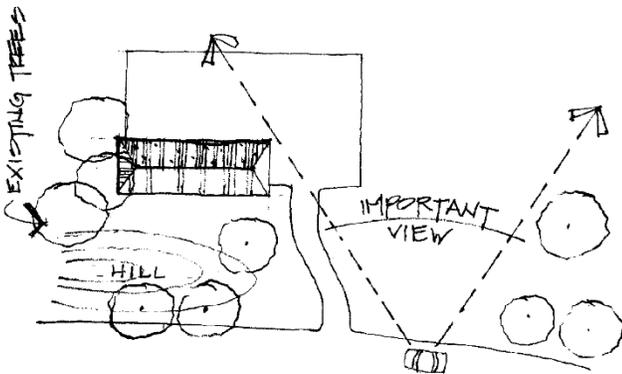
proposed, or when renovations that could affect the appearance of an existing structure are proposed.

- ✓ **Mixed use buildings and projects** - Compatibility could include mixed use developments where feasible to include the historic pattern of services/employment centers with residences above or nearby, to allow enhanced pedestrian opportunities and decreased traffic congestion.



For example, interconnected mixed-use projects allow pedestrians to live and walk to nearby work and entertainment opportunities or to needed goods and services without having to drive distances to do so. This limits traffic congestion and parking demand.

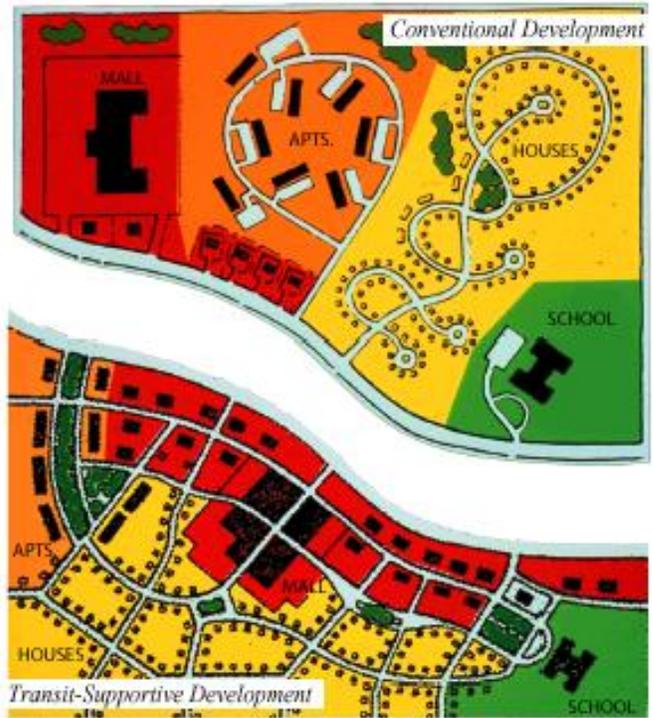
- ✓ **Building placement** – Buildings should be sited so that obstruction of important or priority views from roadways, sidewalks, and parks will be minimized. This can be achieved by taking advantage of topographic changes or existing vegetation.



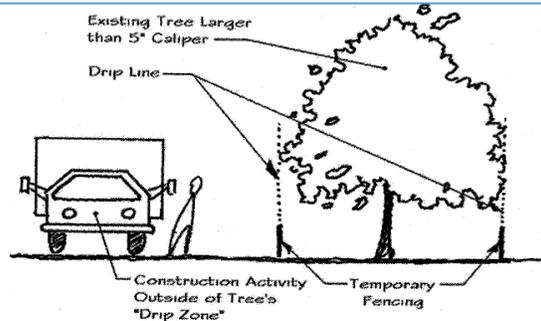
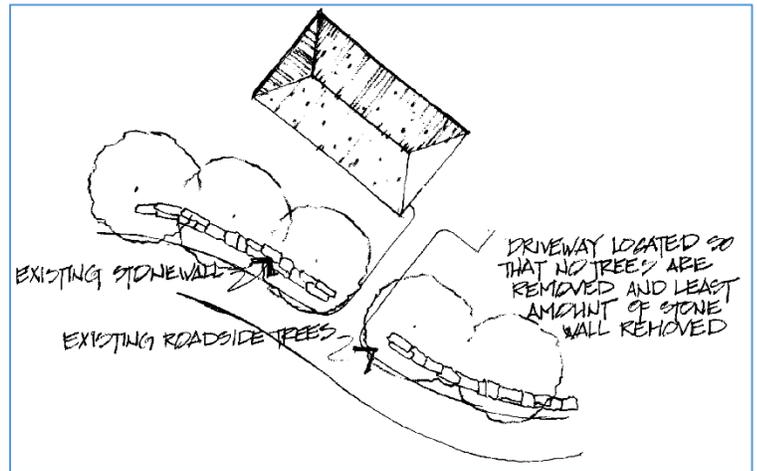
BUILDING LOCATED OUT OF THE WAY OF A BEAUTIFUL VIEW OF THE COUNTRY SIDE, TAKING ADVANTAGE OF A SMALL HILL AND EXISTING TREES TO SCREEN THE BUILDING.

- ✓ **Existing features** – where existing character features occur such as roadside trees, stone walls, tree lines, fencerows (which often have trees and fences of some kind), they should be preserved (or disturbed as little as possible). Such features serve to retain the rural character of roads.

- ✓ **Retain existing trees and vegetation** – When existing trees and/or vegetation are considered to be mature, healthy, and desirable they should be preserved/protected during the construction process.



Source: Andrew Dauby and Elizabeth Flaxer-Zybert



TREE PROTECTION DURING CONSTRUCTION

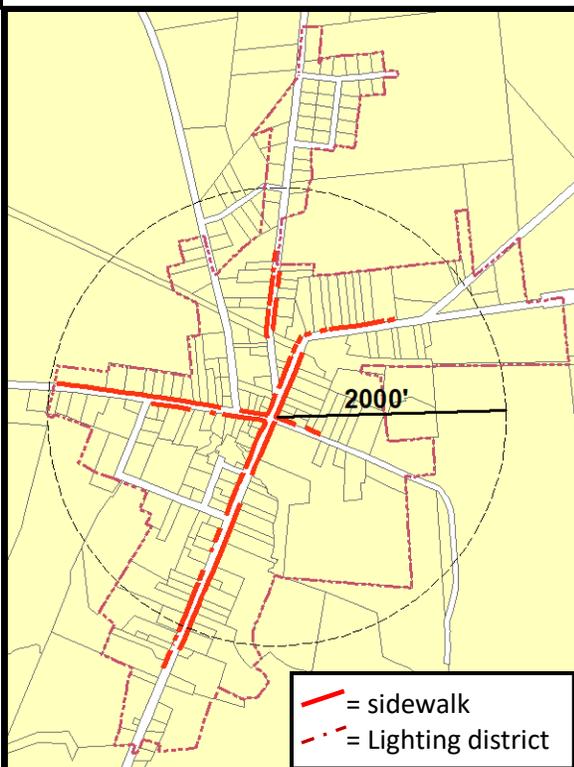
✓ Recommended Walkability standards:

Sidewalks or pedestrian pathways should connect:

- 1) Homes within $\frac{1}{4}$ to $\frac{1}{2}$ mile of most services;
- 2) Elementary schools within $\frac{1}{4}$ to $\frac{1}{2}$ mile of homes;
- 3) Parks within an eighth to $\frac{1}{4}$ mile of homes;
- 4) Town offices within $\frac{1}{4}$ to $\frac{1}{2}$ mile of the community center.

Downtowns should provide a mix of uses such as residential and a balance of retail, commercial stores and services, For example: hair salon, hardware store, pharmacy, coffee shop, ice cream shop, grocery/deli, restaurants, clothing, post office, library.

LaFargeville - Walkability



Pedestrian Friendly Guidelines

Sidewalk Design:

- 4-foot minimum width in residential areas;
- 8 to 15 feet in commercial or main street areas;
- 7 foot minimum height clearance.
- Durable materials (concrete or brick pavers).
- Separate at least 5 feet back (preferably 6 feet) from curb to separate pedestrians from traffic and road spray, allow room for street trees and snow storage, and prevent side slopes at each driveway.
- Meet ADA (Americans with Disabilities Act) requirements.

Sidewalk Locations:

- Both sides along central circulation streets, in commercial districts, near schools, and in residential areas with more than 3 units per acre.
- At least on one side in residential areas with 1 to 2 units per acre.
- Optional one side or wide shoulder in areas with less than 1 unit per acre.

Crosswalks:

- As short as possible with small corner radii.
- About 10 feet wide, well lit, boldly marked with bar stripes or textured surface, and at every major intersection and a selected higher volume mid-block crossings.
- Extend curbs/sidewalks (bumpouts) into parking lanes to shorten crosswalks and increase visibility.
- Handicapped ramps at all intersections.

Traffic:

- Slow speeds to under 30 mph in central locations, especially in higher level pedestrian traffic areas.
- Provide pedestrian signals and eliminate right turn on red at major crossing locations.
- Make pedestrian areas obvious to drivers (using Crosswalk markings) to indicate where walkers can be expected and slowing down is advisable.

Residential Project Considerations

- ✓ **Building setbacks vs build-to lines** - within the Hamlets and other areas in Town, different setbacks could be set depending on the prevalent pattern in the area, in some cases shallower build-to lines that maintain the small historic front yard pattern with larger rear yards should be required to maintain historic residential and business patterns close to the street. In less dense areas where primary buildings are further from the road, larger setbacks could be maintained.
- ✓ **Highway frontage development, vs new roads/streets** - strip development should be discouraged where possible, to maintain traffic carrying capacity of arterial and collector streets. Therefore, new streets or local streets should be used for new development where feasible.

For example, lots that are created one at a time along a main highway can slowly alter the function of the arterial road. As each fronting lot creates a subsequent driveway access, it allows another potential conflict point and reason that traffic must slow down or face either an oncoming automobile or exiting vehicle that may be decelerating or accelerating which ultimately affects traffic flow.

- ✓ **Clustering** – clustering of businesses or homes should be encouraged where feasible to limit strip development and allow open space character and farming activities to be preserved while desired growth occurs.

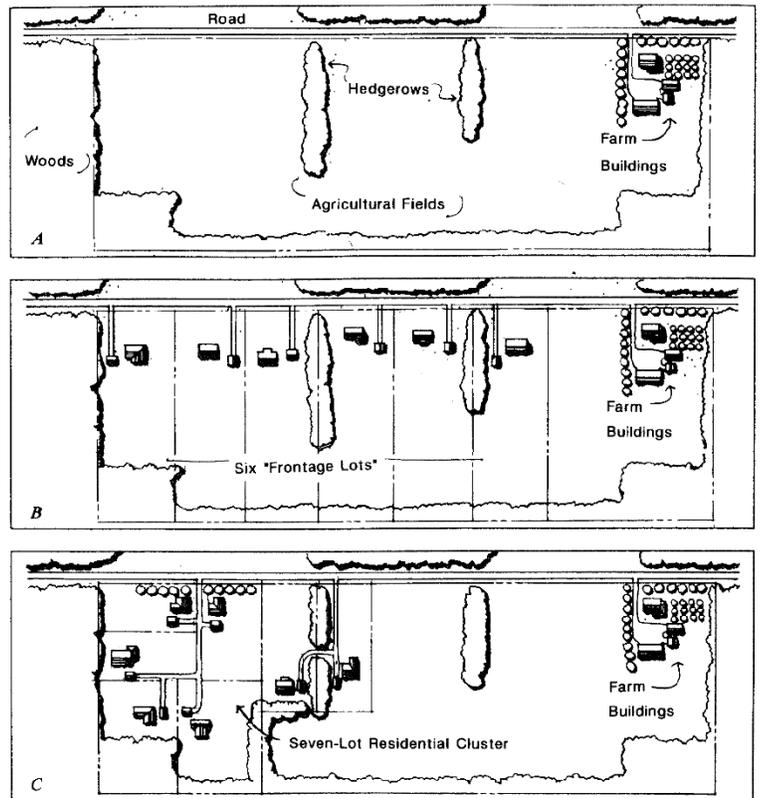


Figure 12-5. On unwooded sites, such as open fields or pastures, it is even more important to encourage (or require) clustering of new development. Even on relatively shallow sites, where there is little opportunity to locate homes far from the road, such as against a distant treeline, clustering principles can help reduce roadside clutter and preserve some open vistas. These three sketches, from *Managing Change: A Pilot Study in Rural Design and Planning* (Doble et al., 1992) show techniques being advocated by the Tug Hill Commission in upstate New York, where the cost of constructing paved subdivision streets operates as a strong disincentive for rural landowners to subdivide their property in any way other than through “strip lots” along existing public roads. This example shows several gravel-surfaced shared driveways, built to standards appropriate for the amount of traffic they must accommodate.

Placing homes near one another also minimizes the installation and maintenance costs of running roads and utilities to them, and allows a better sense of community.

- ✓ **Pedestrian scale or walkable to/from – where feasible**, foster walkable projects that include sidewalks and pedestrian paths, within walking distance from other destinations, and are in scale with village businesses and residential areas.

For example, pedestrian scale typically balances pedestrian and vehicular needs while providing comfortable environments for people to

assemble and associate with others. Community design should be human-scale with services within reasonable distance from one another.

✓ **Soil Conditions influencing development patterns** - based on existing soils, ensure projects address individual septic and drainage issues to limit contamination and off-site impacts.

✓ **Dead-end streets vs loop streets** - dead end streets should only be used to access a limited number of homes (less than twenty), after which a second connection should be provided to an arterial or collector road.

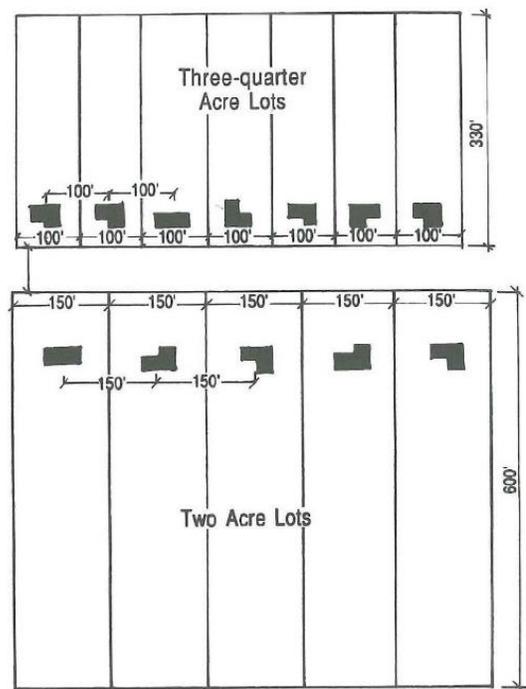
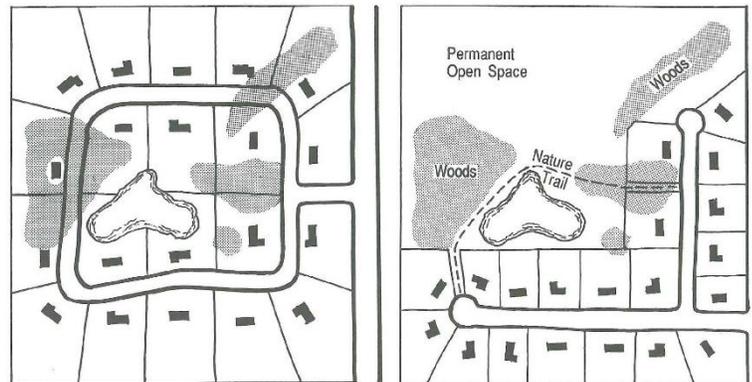
For example, if the single access became blocked by an accident or incident and an emergency occurred in a subsequent house further up the single access road, getting to the 2nd emergency could be delayed or even blocked off entirely for a period of time.

✓ **Hamlet and community lot sizes, smaller vs larger** - where feasible, smaller lots should be required to maintain the historic residential and business density with housing and business patterns close to the street to maintain pedestrian scale development.

✓ **Cost effective services** – Infill projects should be encouraged where services exist, or where possible, municipal services should be laid out in a compact manner to limit future maintenance costs.

✓ **Future infrastructure needs** – Future infrastructure projects should be encouraged to maximize the number of users and should be located within desired growth areas.

✓ **Connections between housing areas** – Residential developments should be connected by internal road to limit trips onto the main traffic artery, also shared driveway accesses are favored over an excessive number of curb cuts. Refer to the curb cut/access management image.



- ✓ **Preserve open space/sensitive lands** - Open space and sensitive lands can be preserved by requiring the project to identify and set aside such areas and allowing smaller house lots in those cases. This improved layout often leads to a more marketable project, with open space areas and trails often that can be shared by the residents.

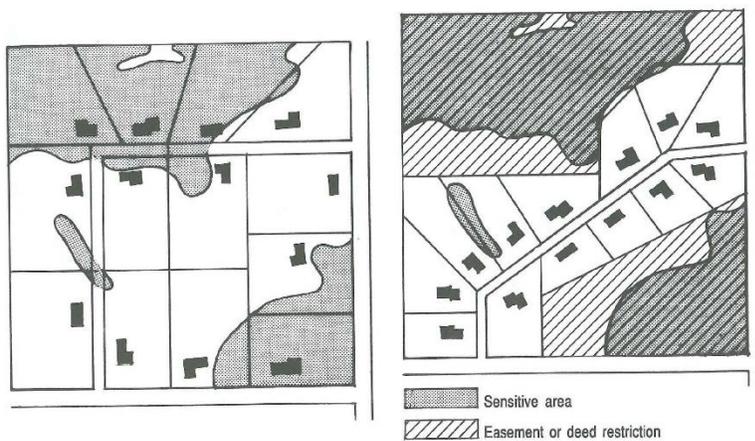


Figure 2-13. Conventional two-acre lot subdivision with homes located on sensitive but buildable land, compared with improved layouts protecting those resource areas, as encouraged by new regulations adopted by the Maine Department of Environmental Protection.

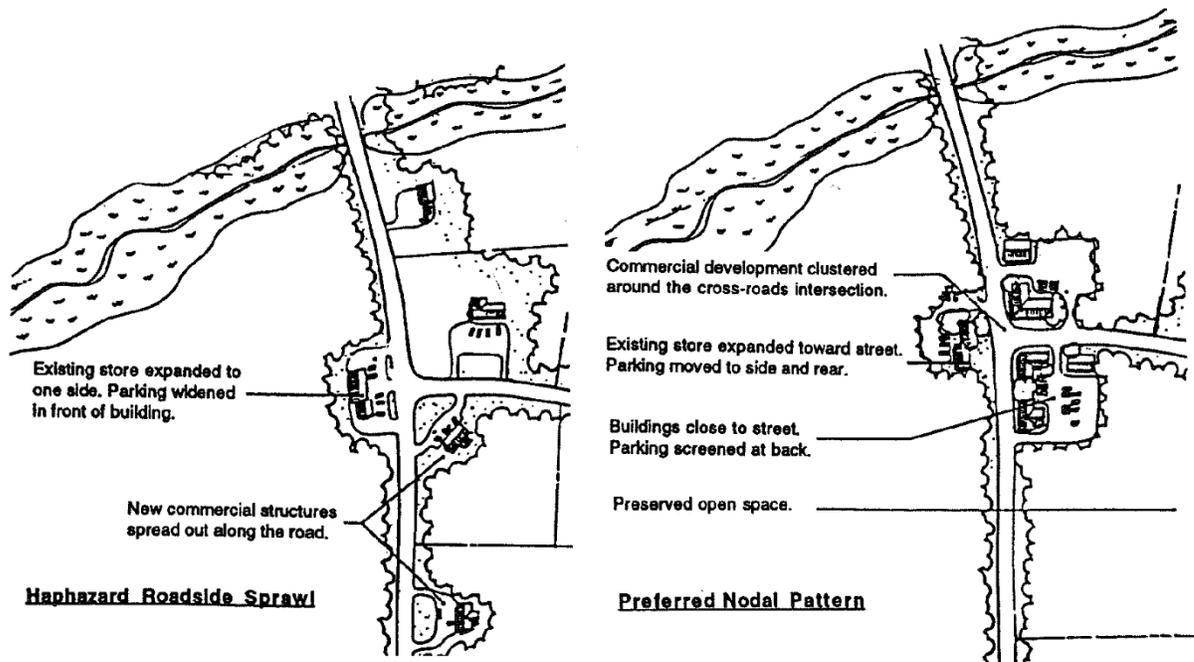
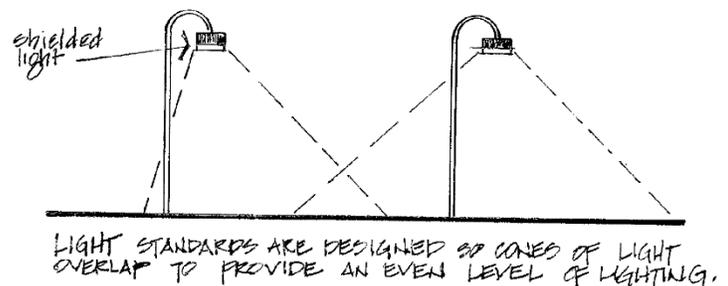


Figure 9-16. Two alternatives for arranging commercial development along a rural highway: strip versus nodes. Within the node, stores are located toward the front of their lots, with interconnected rear parking provision. Source: Dodson Associates.

Commercial Project Considerations

- ✓ **Connections between parking areas/developments & shared access,** - developments should be connected by street access or parking lot connectivity to limit trips onto the main traffic artery, also shared driveway accesses are favored over an excessive number of curb cuts.
- ✓ **Lighting** – Lighting should be used where appropriate, however, over-lighting and excess glare should be avoided, especially on neighboring properties and the public roads. Shielded or cutoff lights should be used to minimize lighting spill-over.

For example, lighting should be controlled in both height and intensity to maintain rural character. Light levels at the lot line should not exceed 0.2 foot-candles, measured at ground level. To achieve this, light fixtures should be fully shielded to prevent light shining beyond the lot lines onto neighboring properties or roadways.



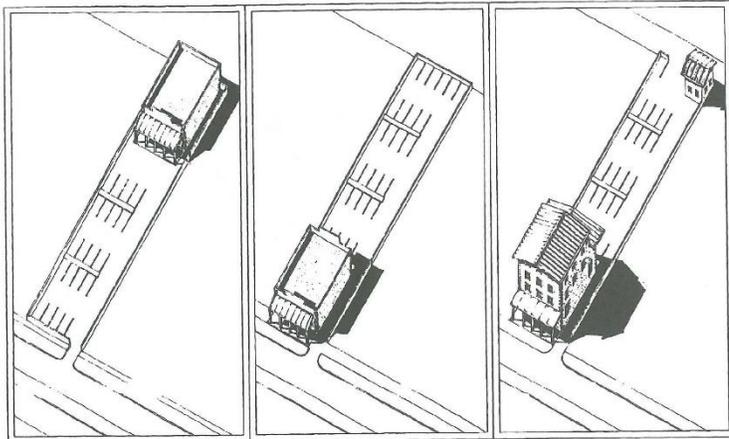
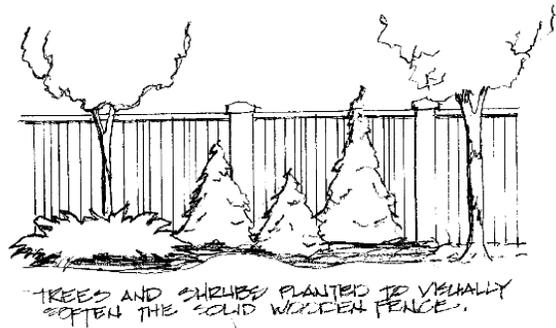


Figure 9-10. Alternative locations for buildings, parking, and access, Davie Settlement, Broward County, Florida. Source: Dover et al., 1990.

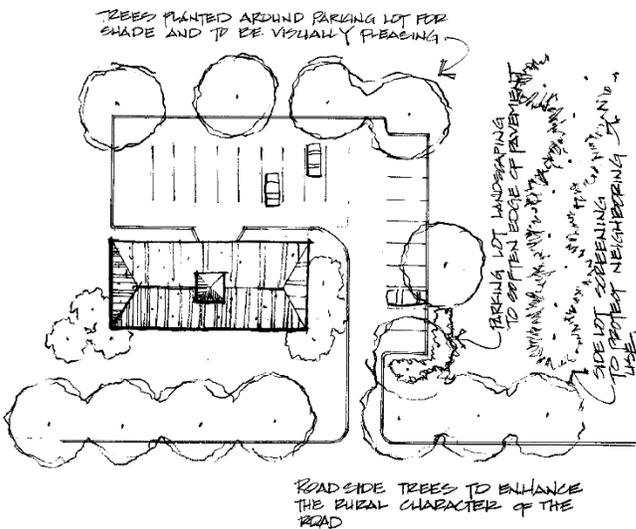
- ✓ **Building setbacks** – Maintain current setbacks in business areas utilizing build-to lines.

For example, setbacks often push new buildings away from roads, fostering a contrasting character and anti-pedestrian pattern than historical patterns of development. Build-to lines require buildings to be placed closer to the street, allow parking to the side and rear, and create a pedestrian friendly streetscape.

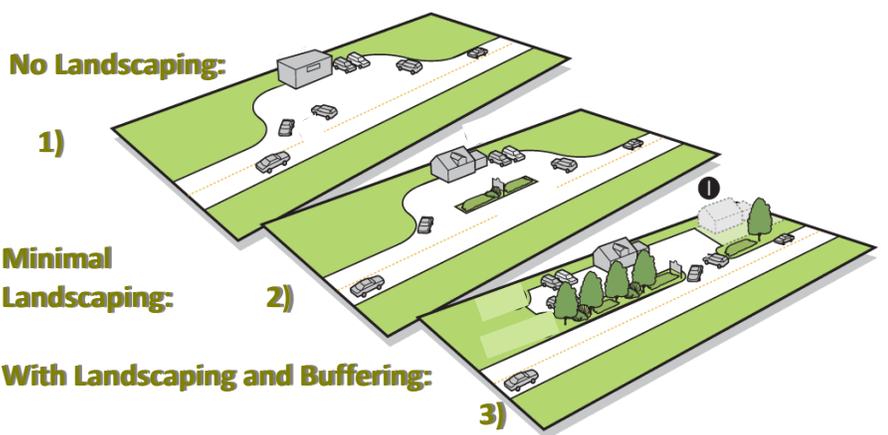
- ✓ **Parking to the side or rear** – the bulk of parking areas should be smaller, distinct areas to the side or rear to allow closer building placement to the road in order to maintain community character, reinforce the visual presence of building as opposed to parked vehicles and the pattern of buildings along the roadside.



- ✓ **Landscaping** – appropriate landscaped buffering should be used to soften parking area edges and buildings, including screening views between uses where needed and partially screen views of parking areas from public roads.



Example of landscaped buffering levels at a site:



✓ **Mixed use development** – mixed use developments should be considered where feasible to include the historic pattern of services\employment centers with residences above or nearby, to allow enhanced pedestrian opportunities and decrease traffic congestion.

✓ **Business hours of operation** - for offices or businesses locating near or within primarily residential areas, consider compatible hours of operation (including hours that parking area lights are used).

✓ **Maximum building heights** – consider building heights compatible with current hamlet business and residential buildings to maintain historic patterns and community character.

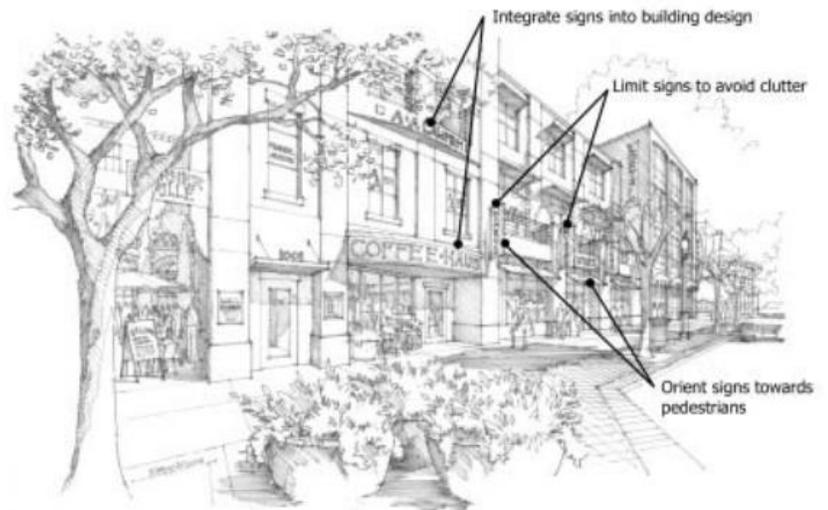
✓ **Signage** – Signage should not all look alike, however, size, type, materials, condition, height and colors are considerations when bearing in mind their design. Effectiveness can be enhanced by proper placement, use of plant materials, and creating distinct views of signage.

A good sign passes three tests:

- 1) conveys its message clearly and quickly;
- 2) Compatible with the structure and its surroundings;
- 3) Promotes the visual image of the community.

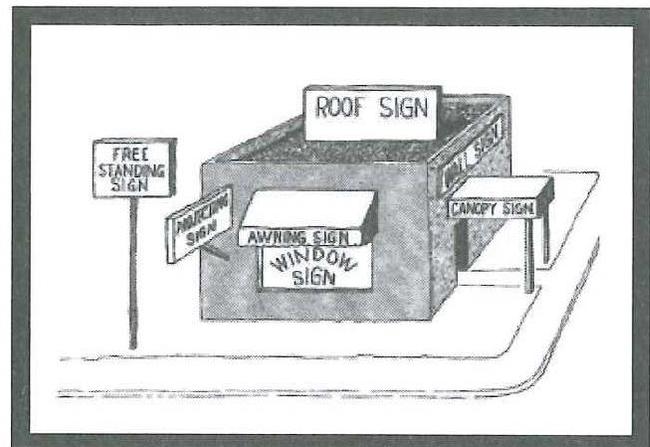
Signage should be an integral part of the building detailing and reflect the particular building style.

Where appropriate, only adequately sized signs should be used (consider letter size and total sign size based upon the speed limit), with a total size allowable limit to ensure efficient signage. Within hamlet areas and slower speed limit



Example of signage integrated into the building design

zones, smaller, and lower signs should be used. Free standing signs should consider lower monument style.



Glare from all signage should be minimized. Internally lit signs (with dark background) could be constructed to limit glare. Except for directional signage, limiting off-premise signs along the Great Lakes Seaway Trail Scenic Byway should be considered (or where sign clutter is a concern, i.e. too many signs present).

Sign lettering should be of a size and style that is clearly legible for passing motorists, but not distracting enough to be hazardous. Target audiences are paramount. Signs along pedestrian facilities should be smaller as pedestrians have more time to view a given sign.

Alternative Energy Project Considerations

As sustainable energy sources become more prevalent and offer options for local energy production, local priorities or standards for such alternative energy projects should be established. Similar to the above Planning Project Considerations, the following considerations should be discussed when updating the zoning criteria used to set standards for their review and potential placement. Alternative energy systems may have an impact on adjacent properties or neighborhood aesthetics, therefore municipalities should review their land use regulations to facilitate opportunities for promoting renewable energy in a way that reflects community values and planning.

Such solar, wind turbine, geothermal, and biomass requirements should address potential impacts to protect the community, its long-term quality of life, and economic value. Therefore, adequate standards should be put in place to allow the Town maintain its local priorities.

√ **Solar Energy** - Solar panels that create electricity from sunlight or use its energy to heat water can be placed on residential roof-tops, accessory buildings, or installed as free-standing, ground-mounted structures. As solar energy systems may have impacts on adjacent properties or neighborhoods, local solar energy standards should address small and large scale

facilities. Large scale solar projects can impact the amount of soils available for agricultural production. Siting on less productive soil areas would be preferred, therefore should be encouraged.

√ **Wind Turbines** – Wind turbines that create electricity from wind can be constructed for different purposes: onsite energy use (residential, small business, or farm), municipal or commercial where energy is sold to the grid. As wind turbines can have impacts on adjacent areas, neighborhoods and the community at large, local turbine standards should address small, community, and large scale commercial projects as community priorities become established.

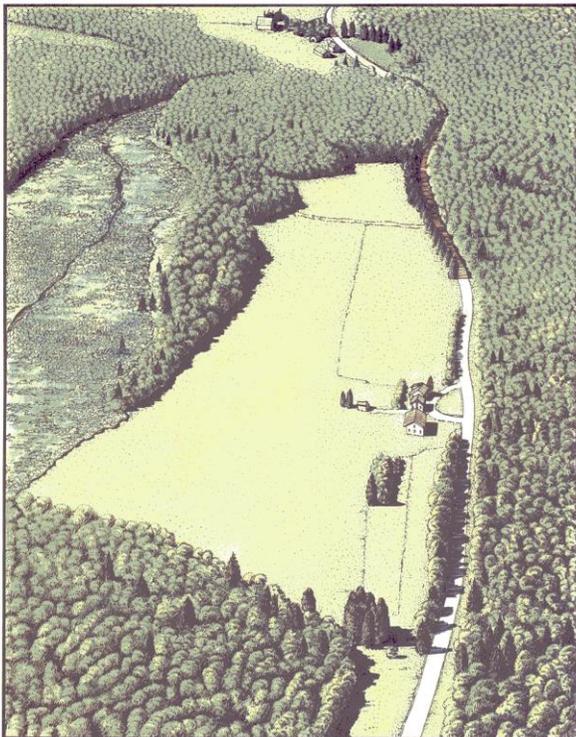
√ **Geothermal** – While resources vary from location to location, new technologies have emerged that are capable of utilizing lower temperatures. Geothermal power can also be harnessed for low intensity heat at relatively shallow depths, which can be used for space and water heating and cooling. As geothermal systems may have impacts on adjacent properties or neighborhoods, local geothermal standards should address small and large scale facilities.

√ **Biomass** – Biomass refers to organic matter that has stored energy through the process of photosynthesis. Such projects can take a variety of forms: electric generation, gasification, anaerobic digestion, or biofuels. Biomass projects can have impacts related to local air quality, water supplies, wastewater disposal, outdoor lighting, facility size, steam emission, noise levels, and traffic levels reinforcing the need for local standards.

Development Forms: Conventional and Creative

During the Public meeting sessions, posters illustrating development scenarios were presented for discussion. They illustrated a site before development, then the same site with conventional development, and that site with a creative form of development. A board with a residential scenario, one with residential and commercial, and a third that illustrated a village or hamlet scenario example such as that found in Redwood were examined by visitors.

Aerial view of site before development



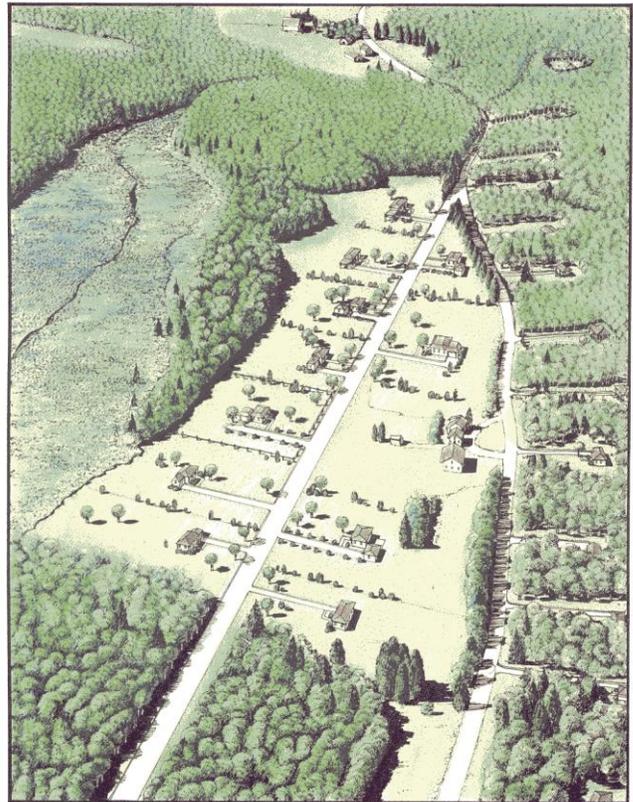
Aerial View of Site C Before Development

SITE DATA

Landuse: Dairy farm on a town road
 Landcover: Field, wetland and forest
 Utilities: No Town water or sewer
 Zoning: Minimum 1 acre lots, 150 ft. frontage

- Farmstead adjacent to scenic town road
- 60 acres of hayfield leased to farmer
- 40 acres of wetland and wildlife habitat

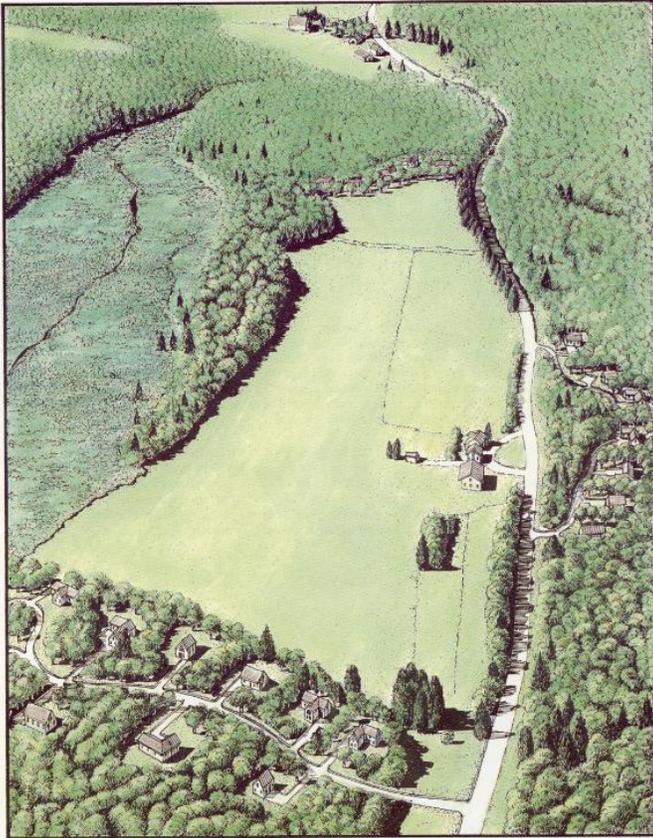
Aerial view of site after potential conventional residential development



Aerial View of Site C After Conventional Development

The above example of conventional development results in the town road being widened and straightened, impacting farmland value and scenery. The developer locates 26 lots on entire acreage affecting most of the farmland and forest. Wetlands and wildlife habitat are then subdivided, thereafter become vulnerable to additional development. Any timber management is then precluded by large lot development.

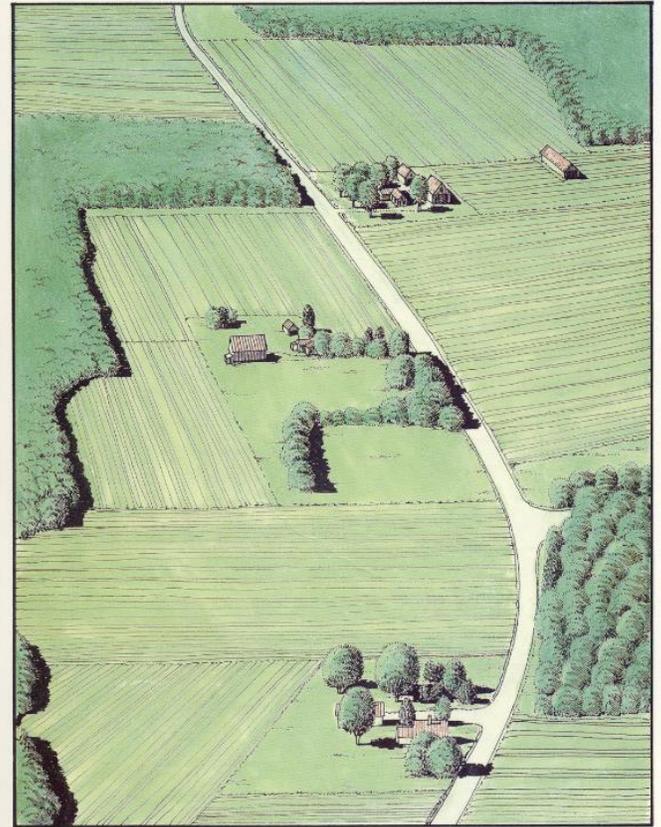
Aerial view of site after potential creative residential development



Aerial View of Site C After Creative Development

This alternate example of Creative Development of the same site results in the town road designated as scenic road and moderate improvements within existing right-of-way. The Town then enacts open space provisions for farmland. Thus, 28 lots are located on 24 acres, saving over 100 acres of farmland and forest. This allows the farmland to continue to be leased by a neighboring farmer.

Aerial view of 2nd site pre-development



Aerial View of Site B Before Development

SITE DATA

Landuse: Cropland & farmhouses on a Scenic State Highway
 Landcover: Fields, woodlands and forest
 Utilities: Town sewer & water available
 Zoning: Highway Commercial, large lot residential

Aerial view of 2nd site after potential Conventional Commercial & Residential Development

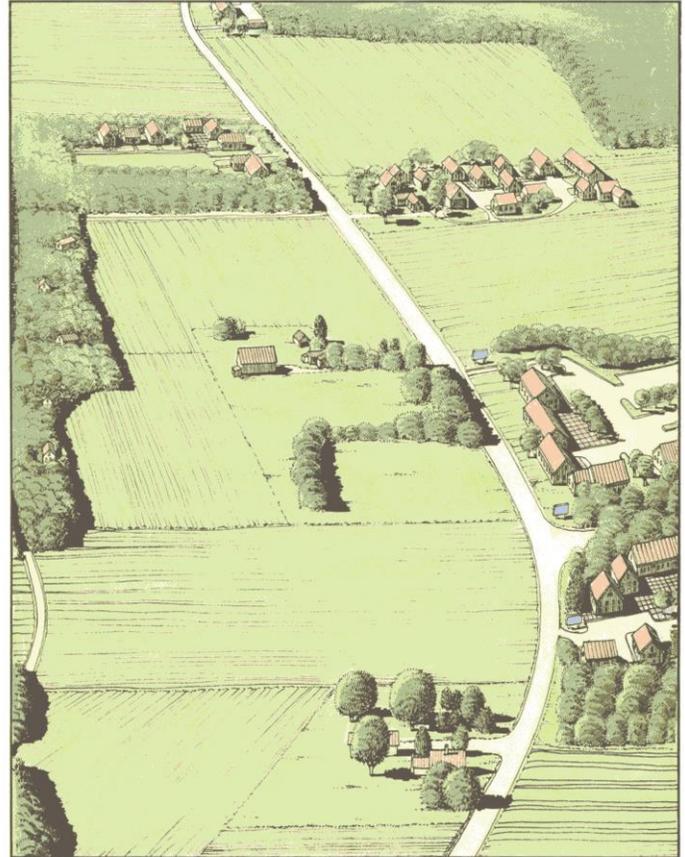


Aerial View of Site B After Conventional Development

Consistent with existing zoning, highway frontage is developed with residential & strip commercial lots. Large signage and parking lots dominate roadside creating visual clutter along highway. This scenario results in a total loss of farmland use, including the loss of rural character. It includes a large lot residential subdivision of farmland behind the commercial strip on new subdivision roads.

The images and excerpts were used from the publication: [Dealing with Change in the Connecticut River Valley: A Design Manual for Conservation](#).

Aerial view of 2nd site after potential Creative Commercial & Residential Development



Aerial View of Site B After Creative Development

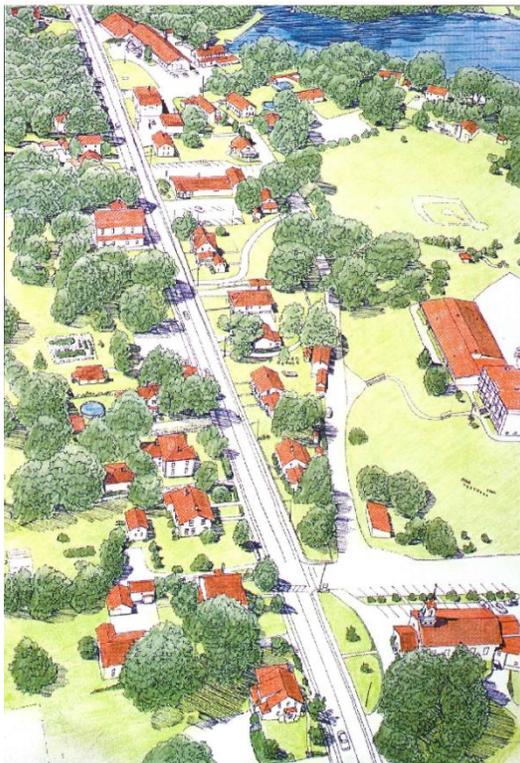
Creative commercial development is clustered in the wooded areas and at major intersections. Signage and lighting controls include underground utilities. Parking and storage are behind buildings. New commercial structures reflect traditional architectural character of the area. Residential development located within clusters also within wooded areas, and at the edges of farmland. Roads would avoid farmland, which would fit along topographic features.

Thus, in the first two examples, the same amount of development is possible (number of lots) while using less than one quarter of the

acreage, with the leftover acreage permitting future farm use or other open space uses.

The next example illustrates a mix of residential, commercial & institutional uses in an Historic Hamlet Center infill development scenario: *South County Design Manual: South County Watersheds Technical Planning Assistance Project - 2001.*

**Historic hamlet center
Prior to infill development**



Historic homes, commercial buildings, brick mills, churches, and other buildings along Main St. Variety in size, shape and styles, unified by the scale and function of Main St.

New gas station /convenience store follows corporate site plan, breaking the street wall by setting the building toward the rear of the lot. Bright colors and corporate logos dominate the canopy, bright lights at night move even further from local tradition.

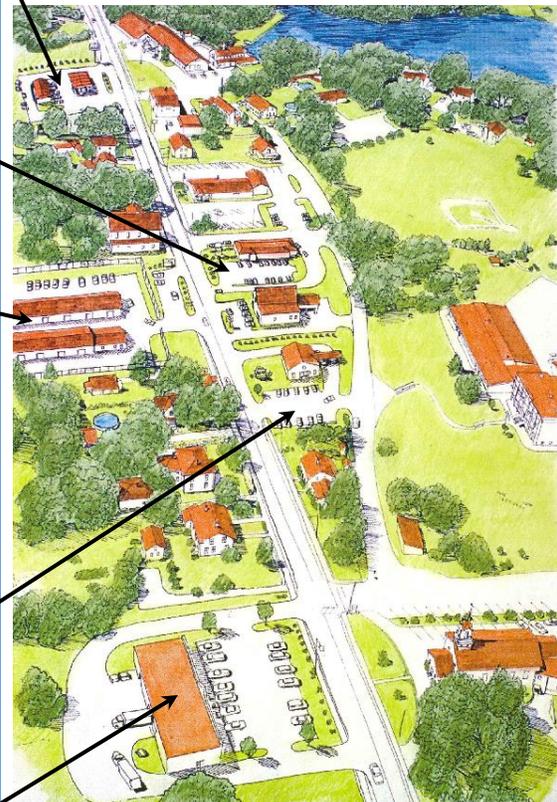
Replacement of historic home with doughnut shop removes an element that was unique to Village in favor of standardized building, parking lot, and drive-thru.

Storage units in the center of the village replace active uses with functional dead zone. Since most access with vehicles, little to no interaction with surrounding uses. Structures tend to be long, flat, and out of scale with the neighborhood.

Conversion of existing buildings from residential to commercial use. Ad hoc redevelopment of individual lots and excessive parking requirements promote inefficient use of space and over-paving.

Historic homes replaced with strip mall. Typical design approach calls for parking in front, service access in rear, two curb cuts for ease of vehicle access. The result is a design that is very convenient for people in cars, but isolated from the surrounding community by asphalt.

**Aerial view of hamlet center after
Conventional Infill Mixed
Development**



Functionally, it is still a 19th century village, with homes, a school, churches, commercial and government uses in close proximity. Currently is a walkable community, with a high degree of livability and sense of place.

The creative infill scenario aerial view and design concepts are described in detail below regarding land uses, access, parking, architecture, landscape, and streetscape components.

Design concept: New uses are required to conform to the visual character and physical patterns of the existing village, rather than the other way around. The emphasis is on maintaining a compact, pedestrian-friendly environment, while meeting the demands of the marketplace for convenient vehicular access and parking.

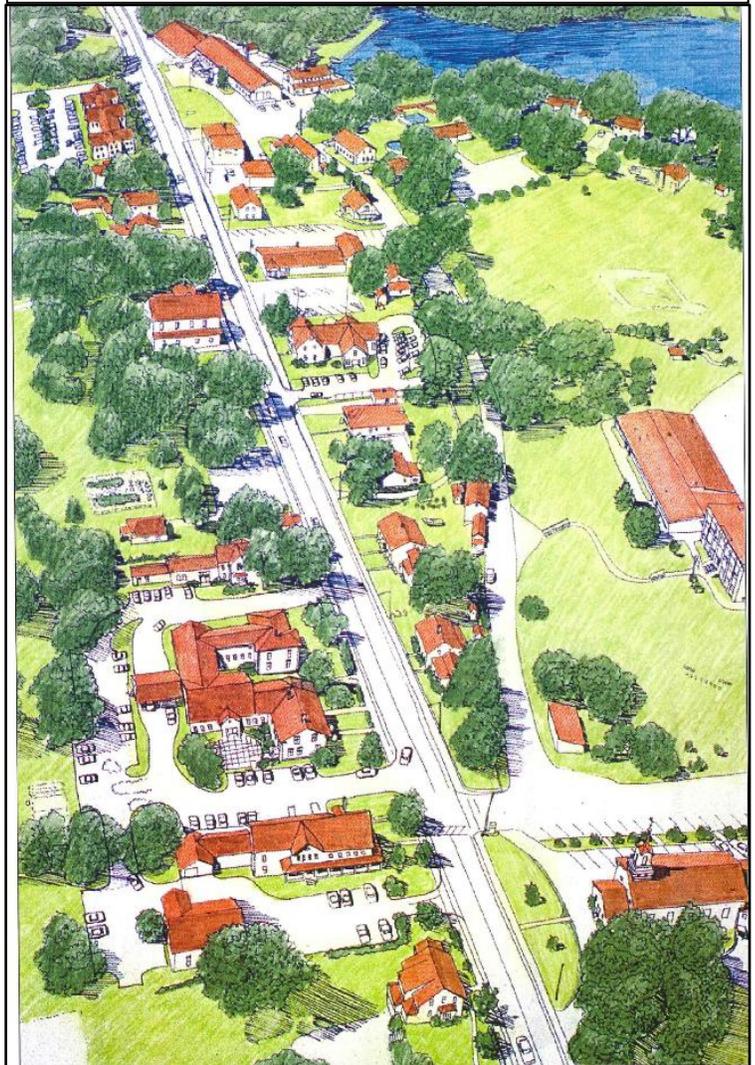
Uses: A mix of residential, commercial, institutional uses would be encouraged, with an emphasis on smaller scale businesses that could fit into existing buildings or new buildings at a compatible scale.

Access: shared curb-cuts between parcels reduce conflicts between cars and pedestrians and improve the streetscape appearance. Driveway connections cross lot lines, which minimizes the number of curb-cuts and allows customers to visit neighboring businesses without pulling back onto Main Street. Drive-thru windows and associated queuing lanes at the rear of buildings allow a needed function, for many modern businesses, while keeping the streetscape pedestrian friendly.

Parking: Parking lots at the side and rear of structures break areas of asphalt up into smaller units more in scale with existing structures.

Architecture: Existing buildings are retained, with additions placed to the rear in compatible architectural styles. Larger uses are accommodated by connecting existing buildings together.

Aerial view of 3rd site after potential Creative Infill Mixed Development



Landscape: Shade trees would be added, and existing trees preserved which would shade new parking lots and reduce their apparent scale. Evergreen shrub plantings screen parking from view.

Streetscape: Improvements to and maintaining sidewalks, addition of benches and trash receptacles, and pedestrian-scale street lights encourage people to walk to and between uses. Existing utilities could be buried, improving the

appearance of the street, opening up views of historic structures and preserving trees.

However, under current zoning in this example (town center), lot size and setback requirements, minimum areas for parking, and open space requirements for each building lot make it hard to build anything without tearing down existing buildings and consolidating lots. While this has slowed development to some extent, typically it's only a matter of time before the rewards to develop outweigh the costs of pursuing this inefficient style of development.

It also means that development is also likely to be driven, not by local residents, but by corporations in some cases looking to expand franchise coverage. The result often does not relate to the existing village in either scale or appearance, which tends to favor automobile over pedestrians, and which almost ensures the loss of historic character and architecture that remains in the village.

Town Recommended Coordination and Implementation Steps

Implementing the Comprehensive Plan requires a series of Town Board policy decisions, program initiatives and local, county and/or state or regional agencies. Some actions recommended in the Plan may also require funding.

This Plan details a series of diverse action steps including administrative actions, zoning amendments and potential policies and programs in some cases, as well as potential capital improvements. Successful implementation of these actions over the

next 5 to 10 years will depend on setting priorities and allocating limited resources – people and funding – to the most important potential projects.

The Town Board will oversee all aspects of implementation of the Plan and will direct work to be done in the future. However, other groups and agencies should be called upon to assist the Board when needed including the Planning Board, Zoning Board of Appeals, Town Zoning Officer, other local officials, organizations and potential new committees as needed along with the Town Board members.

The Town should ensure that copies of this plan are available at the Town Office, in the local library and on the municipal website once finalized including the maps and any appendices.

The Town Board should create and then appoint members to a Comprehensive Plan Implementation Committee or other committees as needed to move the priority actions recommended in the Plan forward. The committee (s) should be comprised of a diverse group of representatives (similar to the Comprehensive Plan Committee) of elected and appointed officials, citizens, and landowners to help guide implementation of the plan. The Town Board should clarify its expectations, time-frames, along with any funding or other assistance such as the County Planning Department staff that it may choose to make available for the committee's implementation efforts.

One suggestion would be to include at least some of the members of the Comprehensive Plan Committee to help the implementation process benefit from a clear or at least familiar understanding of the intent of the Plan as the Planning Process unfolded.

Revisiting and updating the Plan should be considered every five to ten years, beyond that time period if changes have occurred in the community then the Plan may not reflect current priorities.

Further, the Town should review the list of priority actions being implemented on an annual or semi-annual basis to determine the status of efforts, as well as to identify other actions that should be implemented next. At its annual reorganizational meeting, or shortly after, the Town should review the status of implementation of the Plan, and ask for reports from the Zoning Officer, Planning Board and Zoning Board of Appeals regarding any concerns or issues that have arisen over the past year that may need to be addressed in the Plan, zoning regulations or other programs in the Town. This work session could also be devoted to reviewing the action plan, identifying goals reached, retiring completed ones from the action list and adding new actions to be implemented if applicable.

The Town Board should use the table below to help guide its implementation efforts. Some actions are short term and low cost. Others are medium terms while others are long term that may require more time or funding to implement.

The following actions are identified as priorities for the Town to begin work on soon after adoption. The tables constitute a guide for implementing the Plan. They group actions into three or four priority categories. Immediate priority actions should be initiated and completed within the first six months to one year after plan adoption. Secondary priority actions should be completed within the first year to two years of plan adoption. Long term priority actions should be completed within the first three to five years of plan adoption.

One of the first actions, after Plan adoption is to establish a realistic implementation schedule. This schedule should be reviewed annually to evaluate progress and to modify as necessary.

TOWN OF ORLEANS COMPREHENSIVE PLAN IMPLEMENTATION TABLE: 2019-2020				
RECOMMENDED ACTIONS: (in no particular order, except by priority type)		ACTION	LEAD RESPONSIBILITY	CRITERIA FOR EVALUATING SUCCESS
IMMEDIATE PRIORITY (six months to one year)				
ACTION STEP A	Review and revise existing zoning to clarify and simplify where intent may not be clear	Zoning Text amendment	Town Board in consideration of Committee report	Law adopted by Town Board
ACTION STEP B	Update and add definitions within the zoning law to include any current, expected or desired compatible uses.	Zoning Text amendment	Town Board in consideration of Committee report	Law adopted by Town Board
ACTION STEP C	Avoid issuing area variances (in cases with small lots having poor soils for septic placement), where insufficient lot area exists for adequate separation between well and on-site septic system treatment.	Area variances	Zoning Board of Appeals	Approvals will occur only when adequate acreage exists
ACTION STEP D	Update the zoning law to protect existing land uses and encourage compatible development in scale, type and character to enhance the community.	Zoning Text amendment	Town Board in consideration of Committee report	Law adopted by Town Board
ACTION STEP E	Ensure\recommend high-impact commercial or industrial uses to be sited on existing truck routes.	Committee or Planning Board	Planning Board upon recommendation	Criteria adopted by Town Board
ACTION STEP F	Maintain commercial and industrial zones as "nodes" or clusters rather than linear strips along extensive stretches of roadway. A node is point at which roads intersect or a logical central point.	Zoning Map amendment	Town Board in consideration of Committee report	Map amendment adopted by Town Board
ACTION STEP G	Future zoning law applications for special use permit and/or site plan review should incorporate stamped drawings prepared by an engineer or architect to enhance the effectiveness of the review.	Zoning Text amendment	Town Board in consideration of Committee report	Law adopted by Town Board
ACTION STEP H	Address on and off-premise signage to incorporate Town priorities protecting neighborhood and community character. This includes sign size and setback provisions.	Zoning Text amendment	Town Board in consideration of Committee report	Law adopted by Town Board

TOWN OF ORLEANS COMPREHENSIVE PLAN IMPLEMENTATION TABLE: 2020-2022				
RECOMMENDED ACTIONS: (in no particular order, except by priority type)		ACTION	LEAD RESPONSIBILITY	CRITERIA FOR EVALUATING SUCCESS
	SECONDARY PRIORITY (first year to two years)			
ACTION STEP I	Consider updating the zoning district map to protect existing land uses, encourage compatible uses and priority development types to enhance the Town.	Zoning Map amendment	Town Board in consideration of Committee report	Law adopted by Town Board
ACTION STEP J	Catalogue and prioritize any potential water and/or sewer needs for possible funding requests.	Committee or Town Engineer	Town Board upon recommendation	Funding request submitted
ACTION STEP K	Create and implement policies to encourage standards for design and development in priority areas (such as Route 12, LaFargeville, Wellesley Island, Fishers Landing, and Stone Mills).	Zoning Text amendment	Town Board in consideration of Committee report	Law adopted by Town Board
ACTION STEP L	Discuss farm equipment width needs and/or horse and buggy widths with Town Highway Superintendent for upcoming Town road and shoulder projects where wider shoulders may be warranted to enhance safety.	Committee or Town Highway Superintendent	Town Board upon Highway Dept. recommendation	New road shoulders will be wide enough
ACTION STEP M	Consider zoning district and zoning law amendments to incorporate pedestrian friendly site layouts and land use types to enhance walkability within LaFargeville as new developments occur.	Zoning Text amendment	Town Board in consideration of Committee report	Law adopted by Town Board
ACTION STEP N	Draft suitable rural/historic character compatibility techniques including land use, landscaping, lighting, signage, lot coverage and building placement, for possible inclusion in the Zoning Law.	Zoning Text amendment	Town Board in consideration of Committee report	Law adopted by Town Board
ACTION STEP O	Examine options for publicity regarding tourism, waterfront and recreation resources in the Town. Also marketing to prospective residents regarding the excellent school, recreation, businesses and business growth potential including low taxes compared to other communities. Explore cooperative marketing efforts.	Committee or Town Bd	Town Board upon recommendation	Local Chamber of Commerce
ACTION STEP P	Identify historic structures and landmarks to be incorporated in the SEQRA and/or site plan review processes.	Committee or Town Planning Bd	Planning Board upon recommendation	Law adopted by Town Board
ACTION STEP Q	Define hamlet growth areas based upon their ability to support growth.	Zoning Text amendment	Town Board in consideration of Committee report	Law adopted by Town Board

RECOMMENDED ACTIONS: (in no particular order, except by priority type)		ACTION	LEAD RESPONSIBILITY	CRITERIA FOR EVALUATING SUCCESS
<u>SECONDARY\LONG TERM PRIORITY</u> (first year to three to five years)				
ACTION STEP R	Examine commercial areas and any congested roads or highways, determine potential pedestrian needs to locate possible sidewalks or pedestrian pathways, trails or other techniques for enhancing safety while maintaining traffic flow.	Committee or Town Engineer	Town Board upon recommendation	Law adopted by Town Board
ACTION STEP S	Clarify and simplify the process by which existing small businesses can expand in sites zoned for such uses.	Zoning Text amendment	Town Board in consideration of Committee report	Law adopted by Town Board
ACTION STEP T	Consider clarifying the seasonal rental, RV and RV trailer provisions in the zoning law	Zoning Text amendment	Town Board in consideration of Committee report	Law adopted by Town Board
ACTION STEP U	Pursue avenues to address property maintenance, abandoned property safety and beautification (elimination of junk and clutter storage) to maintain or enhance scenic quality in neighborhoods and scenic corridors such as along the Great Lakes Seaway Trail Scenic Byway.	Committee, Zoning Officer or Town Bd	Town Board upon committee or Zoning Officer recommendation	Law or initiatives adopted by Town Board

TOWN OF ORLEANS COMPREHENSIVE PLAN IMPLEMENTATION TABLE: 2023 - 2025

RECOMMENDED ACTIONS: (in no particular order, except by priority type)		ACTION	LEAD RESPONSIBILITY	CRITERIA FOR EVALUATING SUCCESS
<u>LONG TERM PRIORITY</u> (first three to five years)				
ACTION STEP V	Develop a hamlet revitalization program for LaFargeville and other hamlets	Committee, Zoning Officer or Town Bd	Town Board in consideration of Committee report	Law or initiatives adopted by Town Board
ACTION STEP W	Enable low impact small businesses to locate in "mixed-use" zones.	Zoning Text amendment	Town Board in consideration of Committee report	Law adopted by Town Board
ACTION STEP X	Examine existing and potential sidewalk, pedestrian\bike trail, recreation and\or park recreation areas and trails throughout the Town, to attempt to identify gaps and possible future projects and related needs in the community.	Committee or Town Engineer	Town Board in consideration of Committee report	Funding request submitted